

**MEMORANDUM OF AGREEMENT
AMONG
THE FEDERAL TRANSIT ADMINISTRATION,
THE DISTRICT OF COLUMBIA STATE HISTORIC PRESERVATION OFFICER
AND
THE WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY
REGARDING
THE NORTHERN BUS GARAGE RENOVATION PROJECT IN
WASHINGTON, DC**

WHEREAS, the Federal Transit Administration (FTA) plans to provide financial assistance to the Washington Metropolitan Area Transit Authority (WMATA) for the proposed renovation of the Northern Bus Garage, historically known as the Capital Traction Company Car Barn, which is listed on the National Register of Historic Places (NRHP; NR# 13000290, May 22, 2013) (Undertaking) and located at 4701 14th Street, NW; and

WHEREAS, the Northern Bus Garage Renovation Project (Project) consists of the stabilization, restoration, and preservation of the portions of the Northern Bus Garage along 14th Street, NW, including the administration offices and tower, and historic walls on the north and south ends of the building; the demolition of the remaining portions of the historic building and later, non-historic additions; and replacement of the demolished portions with a new building that will be connected to the preserved historic building; and

WHEREAS, FTA has consulted with the District of Columbia State Historic Preservation Officer (DC SHPO) regarding the Undertaking in accordance with 36 CFR Part 800, the regulations implementing Section 106 of the National Historic Preservation Act (54 U.S.C. § 306108); and

WHEREAS, FTA in consultation with the DC SHPO has determined the Undertaking's Area of Potential Effects (APE), as defined in 36 CFR § 800.16(d), as including the entirety of the Northern Bus Garage footprint, and approximately one block of residential or commercial structures along (clockwise starting north) Decatur Street NW, Iowa Avenue NW, Arkansas Avenue NW, Buchanan Street NW, and 14th Street NW, and viewsheds from the intersections of Crittenden Street NW and 15th Street NW facing east, Decatur Street NW, and 15th Street NW facing east, as depicted in Attachment 1; and

WHEREAS, FTA and DC SHPO have applied the criteria of adverse effect pursuant to 36 CFR § 800.5 and determined that the Undertaking will have an adverse effect on the Northern Bus Garage because it will result in the destruction of part of the historic building; and

WHEREAS, WMATA, as a recipient of Federal assistance for the Project, is a consulting party in the Section 106 process pursuant to 36 CFR § 800.2(c)(4) with a responsibility for implementing the terms of this Memorandum of Agreement (MOA) and is invited to sign this MOA as an invited signatory pursuant to 36 CFR § 800.6(c)(2); and

WHEREAS, FTA and DC SHPO invited Uptown Main Street, the Sixteenth Street Neighborhood Association, the Northern Busbarn Neighbors, DC Advisory Neighborhood Commission (ANC) 4C02 and ANC 4C03 to be consulting parties pursuant to 36 CFR § 800.2(c)(5), and consulted with them regarding the effects of the Undertaking on historic properties; and

WHEREAS, in accordance with 36 CFR § 800.6(a)(1), FTA has notified the Advisory Council on Historic Preservation (ACHP) of its adverse effect determination with specified documentation, and the ACHP declined to participate in the consultation pursuant to 36 CFR § 800.6(a)(1)(iii);

NOW, THEREFORE, FTA, the DC SHPO, and WMATA (henceforth referred to as the Signatories) agree that the Undertaking shall be implemented in accordance with the following stipulations in order to take into account the effects of the Undertaking on historic properties.

STIPULATIONS

FTA and WMATA shall ensure that the following measures are implemented.

I. IMPLEMENTATION OF DESIGN PLANS

WMATA will construct the Project according to the design plans included in Attachments 2, 3, and 4. These design plans were determined to be the preferred design through robust Section 106 consultation and public outreach to ensure the following items are met:

- A. New construction illustrated in Attachment 2 will be compatible with the historic Northern Bus Garage; will incorporate projecting and receding elements to decrease the monolithic nature of the new structure along Arkansas and Iowa avenues; and use cladding material and visual patterning to further “break down the scale” of the new building, especially near building entrances and garage doors.
- B. Restoration work will be informed by the *Identification of Historic Fabric Report* included in Attachment 3 and implemented in accordance with the plans and narrative scope of work included in Attachment 4 to ensure that historic fabric from the 1906 to 1959 NRHP Period of Significance will be preserved and the historic portions of the Northern Bus Garage will remain prominent features of the overall Northern Bus Complex. Restoration work includes, but is not limited to, preserving and repairing existing historic fabric, restoring elevation elements that have been replaced with inappropriate elements, replacing inappropriate 1980s windows with historically appropriate replacement windows, and preserving and restoring historic sections of the north and south walls and the original smokestack. As part of its on-going review for DC building permits, the DC SHPO may require minor revisions to the plans in Attachment 4.

II. INSTALLATION OF REPLICA STREETCAR TRACKS

To illustrate and highlight the Northern Bus Garage's original function as a streetcar car barn, WMATA shall install replica streetcar tracks in the area where streetcars used to enter and/or exit from the building along 14th Street, NW, as shown in Attachment 5. If the District Department of Transportation's (DDOT) Public Space Committee does not approve streetcar tracks extending through public space to 14th Street, NW, WMATA will provide information to document the Public Space Committee's decision, and may revise the plans in Attachment 5 to limit the streetcar tracks to WMATA-owned property. Regardless of their extent, the tracks shall be ADA compliant and avoid tripping hazards. The replica streetcar tracks will be installed as part of building construction project and will be fully installed within one week of issuance of the building occupancy permit.

III. INTERPRETIVE SIGNAGE EXHIBITS

- A. In consultation with the DC SHPO and consulting parties, WMATA shall develop and install one (1) to three (3) exterior interpretive signage exhibits and up to five (5) interior interpretive signage exhibits for the building's community room as described in Attachment 6. The exterior interpretive signage exhibits shall focus on the historical and architectural characteristics that qualify the Northern Bus Garage for listing in the NRHP and explain the replica streetcar tracks described in Stipulation II above. The interior community room exhibits may focus on broader historical themes that relate to the role the Northern Bus Garage played in the development of the surrounding neighborhood and community, including, but not limited to, topics such as African-American history, commercial development, and social history.
- B. In developing topics and materials for the interior interpretive signage exhibits, WMATA shall solicit initial input from consulting parties and DC SHPO. WMATA will reach out to additional groups or individuals who are knowledgeable about community history as appropriate in developing the content for the exhibits, as described in Attachment 6.
- C. WMATA, in consultation with DC SHPO and FTA, will determine which topics will be pursued further, based on input received through outreach described in Stipulation III.B. and Attachment 6, and decide how many exhibits will ultimately be installed.
- D. WMATA shall provide full color digital drafts of all exterior interpretive signage exhibits and interior interpretive signage exhibits to the consulting parties and DC SHPO for review and comment in accordance with Attachment 6.
- E. Once the content, design, and location are approved by DC SHPO in writing, WMATA shall prepare and install the signage in the approved locations within thirty days of issuance of the building occupancy permit.

IV. REVISIONS TO THE PROJECT

If WMATA refines the design of the Project in a manner that may result in additional or new effects on historic properties, WMATA will notify FTA and the DC SHPO of such changes. Before WMATA takes any Project action that may result in additional or new effects on historic properties, WMATA, FTA, and DC SHPO will consult to determine the appropriate course of action.

V. UNANTICIPATED DISCOVERIES

A. Archaeological Resources and Human Remains

1. In the event that a previously unidentified archaeological resource and/or suspected human remains are discovered during ground disturbance activities, all construction work involving subsurface disturbance will be halted in the area of the resource and in the surrounding area where further subsurface remains can reasonably be expected to occur.
2. WMATA shall notify the DC SHPO's District Archaeologist in writing via email and by telephone immediately.
3. The DC SHPO's District Archaeologist shall conduct a site visit within two working days (48 hours), if possible.
4. DC SHPO will contact the Metropolitan Police Department (MPD) and the DC Office of the Chief Medical Examiner (OCME) if suspected human remains are present per OCME protocols under DC Statute DC ST S 5-1406.
5. WMATA, FTA, and DC SHPO will consult to determine whether the resource is eligible for listing in the NRHP, and if so, whether adverse effects can be avoided or minimized.
6. If the resource is determined NRHP-eligible and adverse effects cannot be avoided, WMATA will propose a Treatment Plan to mitigate adverse effects. Upon concurrence by DC SHPO and FTA on the effects and Treatment Plan, WMATA will carry out the Treatment Plan.
7. Documentation, evaluation, and execution of the Treatment Plan will be undertaken by archaeology professionals meeting the requirements of Stipulation VI, comply with District guidelines for archaeology, and be conducted according to an archaeological work plan approved by the DC SHPO.

B. Architectural and Historic Built Environment Resources

1. If, in the course of implementing the Project, unforeseen and potentially adverse effects occur to above-ground historic properties within the APE, WMATA shall immediately

halt all construction work within fifty (50) feet of the unforeseen effect and take all reasonable measures to avoid or minimize further unforeseen effects. WMATA shall notify FTA and DC SHPO of the issue as soon as practicable, but no later than 3 days following the unforeseen effect.

2. WMATA shall ensure that an architectural historian or historic architect meeting the requirements of Stipulation VI investigates the work site and the historic property within seven (7) days. Following the investigation, WMATA shall forward to FTA and DC SHPO an Assessment of Effects Report to the historic property and proposed Treatment Plan to resolve any adverse effects on historic properties. Upon agreement with the Effects Report and Treatment Plan by DC SHPO and FTA, WMATA will carry out the Treatment Plan.
3. At the conclusion of this consultation, WMATA will provide all parties that participated in the discovery consultation a written summary of the consultation and its resolution. This summary may be transmitted to the participants via e-mail.

VI. PROFESSIONAL QUALIFICATIONS

WMATA shall ensure that all historic preservation and archaeological work performed by WMATA or on its behalf pursuant to this MOA shall be accomplished by or under the direct supervision of a person or persons who meet(s) or exceed(s) the pertinent qualifications in the Secretary of the Interior's Professional Qualification Standards (48 FR 44738-9) in those areas in which the qualifications are applicable for the specific work performed.

VII. MONITORING AND REPORTING

Each year following the execution of this MOA until it expires, is fulfilled, or is terminated, WMATA shall provide the signatories a summary report detailing work undertaken pursuant to the MOA. Such report shall include a summary and update on work being carried out in accordance with relevant stipulations, any scheduling changes proposed, any problems encountered, any disputes or objections received, and related topics. WMATA shall provide the annual report to the Signatories on or before the date of execution of the MOA.

VIII. DISPUTE RESOLUTION

Should any Signatory object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FTA shall notify the other Signatories and consult with such party to resolve the objection. If FTA determines that such objection cannot be resolved, FTA will:

- A. Forward all documentation relevant to the dispute, including FTA's proposed resolution, to the ACHP. The ACHP shall provide FTA with its advice on the resolution of the objection within thirty (30) calendar days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FTA shall prepare a written response that takes into account any timely advice or comments regarding the dispute from the ACHP and

signatories and provide them with a copy of this written response. FTA will then proceed accordingly.

- B. If the ACHP does not provide its advice regarding the dispute within the 30-day time period, FTA may make a final decision on the dispute and proceed accordingly. Prior to reaching such a decision, FTA shall prepare a written response that takes into account any timely comments regarding the dispute from the Signatories and provide the Signatories and the ACHP with a copy of such written response.
- C. FTA and WMATA's responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute remains unchanged.

IX. AMENDMENTS

This MOA may be amended when such an amendment is agreed to in writing by all Signatories. The amendment will be effective on the date a copy signed by all Signatories parties is filed with the ACHP. Revisions to any Appendix to this MOA determined to be non-substantive by the Signatories will not require an amendment to the MOA but must be agreed to in writing by the Signatories.

X. TERMINATION

If any Signatory determines that the terms of this MOA will not or cannot be carried out, that party shall immediately consult with the other Signatories to attempt to develop an amendment per Stipulation IX, above. If within 30 days, or another timeframe agreed to by all Signatories, agreement on an amendment cannot be reached, any Signatory may terminate the MOA upon written notification to other Signatories.

If the MOA is terminated, and prior to work continuing on the Undertaking, FTA must either: (a) execute another MOA pursuant to 36 CFR § 800.6; or (b) request, take into account, and respond to the comments of the ACHP pursuant to 36 CFR § 800.7. FTA shall notify the signatories as to the course of action it will pursue.

XI. GENERAL PROVISIONS

- A. Counterparts; Electronic Signature
This MOA may be executed in two or more counterparts, each of which shall be deemed an original, but all of which together shall constitute one and the same instrument. This MOA may be signed electronically.
- B. Distribution of MOA
Within one (1) week of the last signature on this MOA, FTA shall provide each Signatory and consulting party with one high quality, legible, full color, electronic copy of the fully-executed MOA and all of its attachments integrated into a single document. Internet links will not be used as a means to provide copies of attachments since links to web-based

information often change. If the electronic copy is too large to send by e-mail, WMATA shall provide a copy of this MOA as described above, on a flash drive, compact disc, or other suitable, electronic means.

XII. DURATION

This MOA will expire if its terms are not carried out within ten (10) years from the date of execution, or when FTA determines that all stipulations have been satisfactorily fulfilled. WMATA shall notify FTA when the project is completed and there are no further opportunities for unanticipated discoveries as described in Stipulation V above. Prior to expiration, FTA may consult with the Signatories to reconsider the terms of the MOA and amend it in accordance with Stipulation IX.

Execution of this MOA and implementation of its terms evidences that FTA has taken into account the effects of the Undertaking on historic properties and afforded the ACHP an opportunity to comment.

**SIGNATURE PAGE
MEMORANDUM OF AGREEMENT
REGARDING
THE NORTHERN BUS GARAGE RENOVATION PROJECT
WASHINGTON, DC**

SIGNATORY

FEDERAL TRANSIT ADMINISTRATION

By: _____

Date: _____

Terry Garcia-Crews
Regional Administrator, Region III

**SIGNATURE PAGE
MEMORANDUM OF AGREEMENT
REGARDING
THE NORTHERN BUS GARAGE RENOVATION PROJECT
WASHINGTON, DC**

SIGNATORY

DISTRICT OF COLUMBIA STATE HISTORIC PRESERVATION OFFICER

By: 

Date: 12/15/2021

David Maloney
District of Columbia State Historic Preservation Officer

**SIGNATURE PAGE
MEMORANDUM OF AGREEMENT
REGARDING
THE NORTHERN BUS GARAGE RENOVATION PROJECT
WASHINGTON, DC**

INVITED SIGNATORY

WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY

By: Andrew Off
E013349 WMATA

Digitally signed by Andrew
Off E013349 WMATA
Date: 2021.12.10 09:57:04
-05'00'

Date: _____

Andrew B. Off
Executive Vice President, Capital Project Delivery

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Attachment 2: Building Elevations and Perspective Views

Attachment 3: WMATA Northern Bus Garage: Identification of Historic Fabric Report

Attachment 4: Restoration Narrative Scope of Work, Elevations and Plans

Attachment 5: Plans for Replica Streetcar Track Installation

Attachment 6: Interpretive Signage Exhibits

ATTACHMENT 1
AREA OF POTENTIAL EFFECTS
NORTHERN BUS GARAGE RENOVATION PROJECT
MEMORANDUM OF AGREEMENT



Area of Potential Effects —————

ATTACHMENT 2
BUILDING ELEVATIONS AND PERSPECTIVE VIEWS
NORTHERN BUS GARAGE RENOVATION PROJECT
MEMORANDUM OF AGREEMENT

Northern Bus Garage

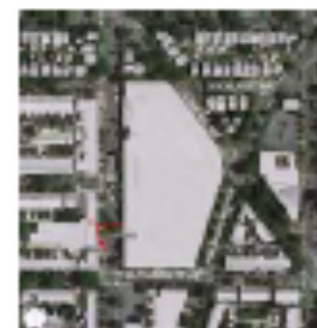
Proposed Perspective Views



SOUTHWEST VIEW LOOKING NORTHEAST ALONG 14TH STREET

Northern Bus Garage Proposed Perspective Views

VIEW LOOKING SOUTHEAST ALONG 14TH STREET AT ENTRY



Northern Bus Garage

Proposed Perspective Views



VIEW LOOKING SOUTHEAST ALONG 14TH STREET

Northern Bus Garage

Proposed Perspective Views



VIEW LOOKING NORTHWEST ALONG IOWA

Northern Bus Garage

Proposed Perspective Views



VIEW LOOKING SOUTHWEST ALONG ARKANSAS

Northern Bus Garage

Proposed Perspective Views



VIEW LOOKING WEST ALONG BUCHANAN AT SOUTHEAST CORNER

ATTACHMENT 3
IDENTIFICATION OF HISTORIC FABRIC REPORT
NORTHERN BUS GARAGE RENOVATION PROJECT
MEMORANDUM OF AGREEMENT



1914 Photograph of the Northern Bus Garage looking southeast (DC History Center)

WMATA NORTHERN BUS GARAGE: IDENTIFICATION OF HISTORIC FABRIC REPORT

Informing the Treatment of the Existing Structure and
Design of the Replacement Bus Garage

February 2020

**BEYER
BLINDER
BELLE**

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Introduction

The Northern Bus Garage is listed in the National Register of Historic Places (NRHP) (listed in 2013) and the D.C. Inventory of Historic Sites (listed in 2012). The building, located at 4615 14th Street NW and formerly known as the Capital Traction Company Car Barn or the Decatur Streetcar Barn, was originally constructed by the Capital Traction Company in 1906. Fully converted from a streetcar barn to a bus garage in 1959 and transferred to WMATA in 1966, the structure is a vital storage and maintenance facility for WMATA's bus transportation services. The original building, designed in the Italian Renaissance Revival style, is a one-story brick masonry building with partial basement level, the length of which spans two city blocks. The building appears to be two complementary masses; one being a two-story structure used as administrative offices and the other housing the repair shops and garage, which features a three-story tower. The building was significantly altered during renovation work completed in 1987-1992. During this time, the southern and eastern elevations of the building were enveloped in a one-story addition with rooftop parking. Decatur Street, to the north, was enclosed and substantial demolition to the roof, interior columns, and basement floor slab also occurred. Additionally, there were many alterations to the administrative offices and the original building elevations.

Current operational and programmatic challenges require that the bus garage be rebuilt while preserving the historic 14th Street façade of the building. It is important that the Northern Bus Garage Replacement Project (the project) meet WMATA's goals of modernization, sustainability, increased community integration, and flexibility for the future needs of electric buses while preserving the historic fabric that retains integrity and expresses the significance of the building.

FTA-funded projects undertaken by WMATA are subject to Section 106 of the National Historic Preservation Act (NHPA), requiring Federal agencies take into account the effects of their undertakings on historic properties and, if the project is determined to have an adverse effect, afford the Advisory Council on Historic Preservation (ACHP) the opportunity to comment on such undertakings. The Section 106 process was initiated in April 2019, and the undertaking was determined to have an adverse effect by FTA and the DC State Historic Preservation Office, although the ACHP declined to participate in the consultation. The project also requires DC Historic Preservation Review Board (HPRB) review and approval. Through the Section 106 process, the FTA has determined that mitigation will be recorded in a Memorandum of Agreement.

History and Significance of the Building

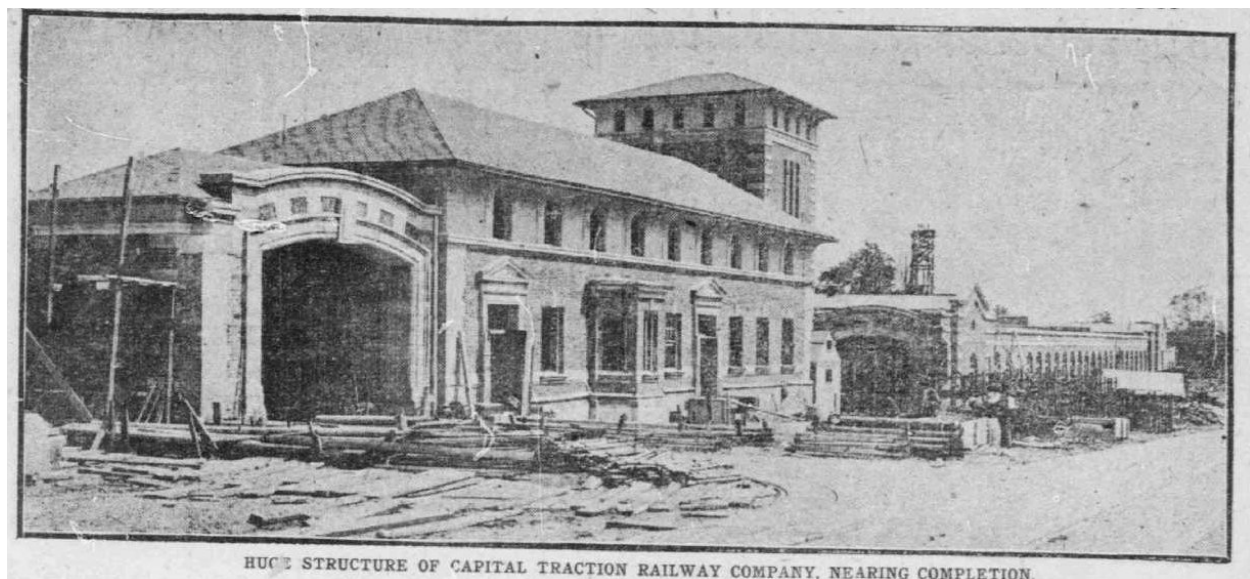
The bus garage was constructed in 1906 to serve as a streetcar storage and maintenance facility and house administrative offices for the Capital Traction Company. (See **Figure 1**) The building was designed by architecture firm Wood, Donn and Deming and was built by construction firm Richardson and Burgess, opening in 1907. In 1926, the basement portion of the building was leased to the Washington Rapid Transit Company for bus maintenance and storage. Between 1956 and 1962, all D.C. streetcar lines were eliminated or converted to bus routes. In 1959, the building was converted to a bus garage, and ownership was transferred to WMATA in 1966.¹

¹ National Register of Historic Places, Capital Traction Company Car Barn, Washington, D.C., National Register #13000290

The Northern Bus Garage building was listed in the D.C. Inventory of Historic Sites in 2012, and in the NRHP in 2013 under Criteria A and C for its architectural and historic significance.² It is considered an outstanding example of Italian Renaissance Revival design for its building type and is directly associated with the streetcar system, a public transportation system that helped develop and determine development patterns of the District of Columbia.³ The building is also eligible for designation under the multiple-property document Streetcar and Bus Resources of Washington, D.C. 1862-1962. According to the multiple property documentation form, to remain eligible under Criterion C, the building must retain its high-style architectural design as well as its original form or shed-like appearance and the streetcar entry openings.⁴

The building's period of significance is from 1906-1959, spanning the period when it served as a streetcar barn.⁵ The period of significance ends when it was converted to a bus garage. Since 1959, many significant alterations have been made to the building.

Figure 1. 1906 photograph of the car barn and administrative offices during construction (Washington Times)



² Under NRHP Criterion A, properties are eligible for listing if they are associated with events that have made a significant contribution to the broad patterns of our history. Under NRHP Criterion C, properties are eligible for listing if they embody the distinctive characteristics of a type, period, or method of construction, or represent the work of a master, possesses high artistic values, or represent a significant and distinguishable entity whose components may lack individual distinction. Under the criteria for the DC Inventory, the property is eligible for designation based on the following values: history and architecture and urbanism.

³ National Register of Historic Places, Capital Traction Company Car Barn, Washington, D.C., National Register #13000290

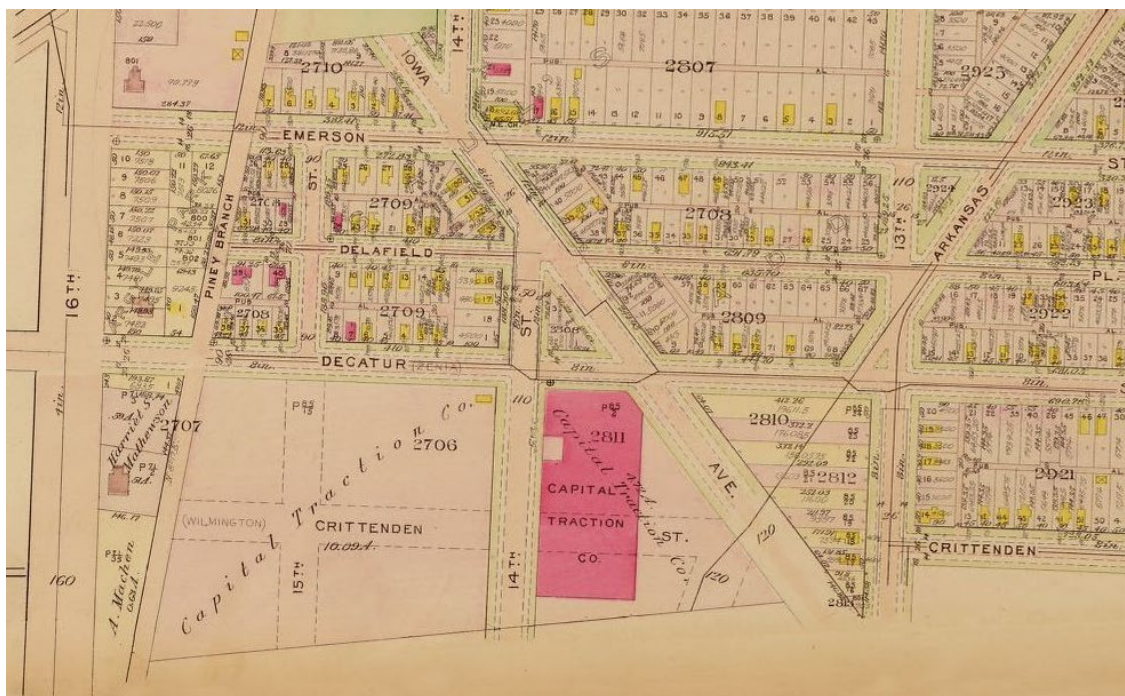
⁴ National Register of Historic Places, Streetcar and Bus Resources of Washington, D.C. Multiple Property Listing, Washington, D.C., #64500948

⁵ National Register of Historic Places, Capital Traction Company Car Barn, Washington, D.C., National Register #13000290

Physical Description and Chronology of Development

The Northern Bus Garage site is bounded by 14th Street to the west, Decatur Street to the north, Iowa Ave to the northeast, Arkansas Ave to the southeast, and Buchanan Street to the south. The main façade of the building faces 14th Street, and therefore, the west façade is the most decorative. As originally constructed, the brick masonry car barn measured 537 feet (north-south) by 208 feet (east-west), occupying nearly half of the site on Square 2811 and a portion of Square 2815. As platted, the two squares were intended to be divided by Crittenden Street. However, because of the construction of the car barn, the road was never laid, and the squares remained joined. The 1911 Baist Real Estate Map shows the original footprint and surrounding streets of the garage. It is interesting to note the residential character of the neighborhoods to the north of the garage and that the Capital Traction Company owned the squares west of 14th Street, yet the area was not developed at the time. (See **Figure 2**)

Figure 2. 1911 Baist Real Estate Map; Decatur Streetcar Barn is the pink building labeled as the Capital Traction Co. (Library of Congress)



The building was designed to look like two complementary masses: a two-story structure housing the administrative offices, featuring a hipped roof with overhanging eaves, and a two-story car barn and repair shop, characterized by a grand three-story tower with a clerestory. As designed and constructed, the garage consisted of an upper (main level) entered along 14th Street and a partially excavated lower (basement) level, accessed from the south elevation of the building. Exterior character-defining features included brick walls accented with stone belt courses, quoining, and keystones; shallow-pitched hipped roofs of the tower and administrative offices, and bracketed eaves. (See **Figure 3**) The garage and repair shop featured a flat roof with a front gable parapet and several large skylights. The building originally

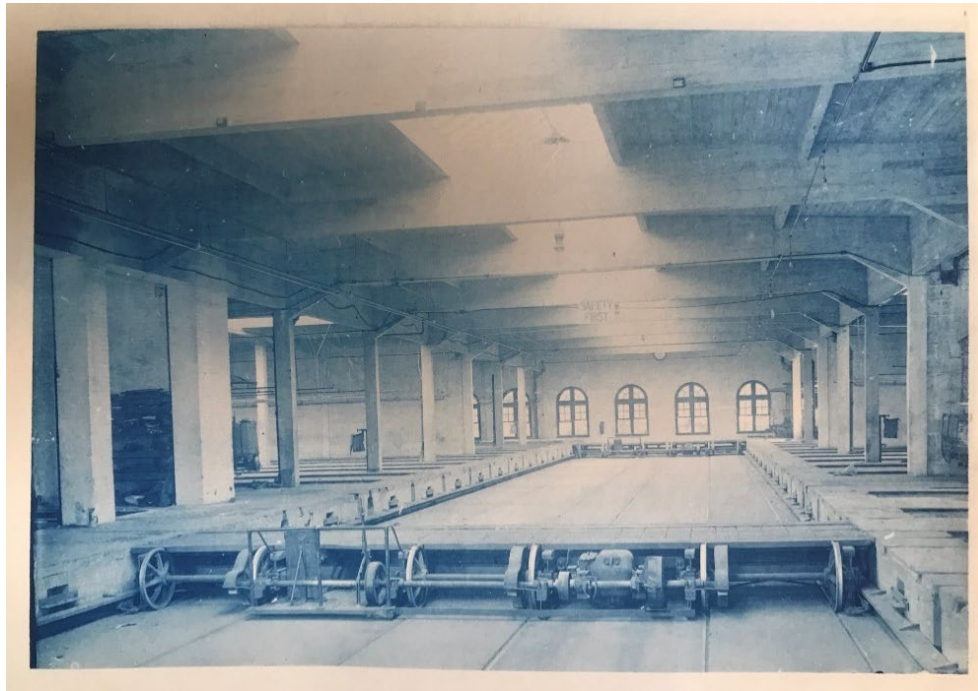
featured three streetcar entrances and exits on the west façade: two facing west and framing the administrative offices and one facing north, immediately adjacent to the tower. Arched window openings on all elevations provided light to the garage and repair shop. The administrative offices, tower, and several projecting pavilions along the north and west elevations featured rectangular windows.

The interior of the car barn is formed by the concrete columns and roof structure. Skylights and the arched window openings provided plentiful daylight. The upper level featured two transfer tables, allowing for the efficient mobility and storage of the street cars. The transfer tables ran parallel to each other from the front (west) to rear (east) elevations of the building. (See **Figure 4**)

Figure 3. 1914 exterior photograph of the northwest corner of the streetcar barn showing the administrative offices, tower, and two of the three original streetcar openings. The north elevation features arched window openings and hipped roof pavilions at the center and western corner of the elevation (DC History Center)



Figure 4. 1914 interior photograph of the Decatur Streetcar Barn showing the transfer table in the foreground and the skylight above (DC History Center)



In 1926, the Washington Rapid Transit Company, established in 1921, leased the lower level of the garage from the Capital Traction Company to use for buses. According to the NRHP nomination, a one-story addition was added to the east elevation of the building at this time to provide storage facilities for the buses. The addition is visible in the 1959 Sanborn map and a 1974 aerial photograph of the bus garage. (See **Figure 5** and **Figure 6**) It is possible that the 1926 addition was expanded after 1959, as it appears slightly larger in the 1974 photograph.

Presumably, many interior alterations were made circa 1959 when the streetcar barn was fully converted to a bus garage, however, the streetcar openings along the west façade continued to be used as bus entries and exits to the garage and repair shops. It is likely that the transfer tables and bays for the streetcars were infilled. Boring samples completed in December 2019 have revealed that partial track infrastructure is extant, although encapsulated in concrete infill. Currently, no documentation has been found that illustrates the interior changes that occurred during this time. Exterior photographs indicate that an additional bus opening was added on the west façade between 1949-1962, immediately adjacent to the north facing opening to the south of the tower. The opening was cut within the pedimented projection, requiring the removal of an arched window opening. A molded cast stone surround, complementing the surrounds of the original openings, was installed. Photographs from 1974 also indicate that an arched window opening at the southern end of the west façade, to the right (south) of the pedimented parapet, was changed to a doorway.

Figure 5. 1959 Sanborn map showing the garage (then owned by the Capital Transit Company) and the 1926 addition at the east side (Capital Traction Company Car Barn National Register Nomination Form)

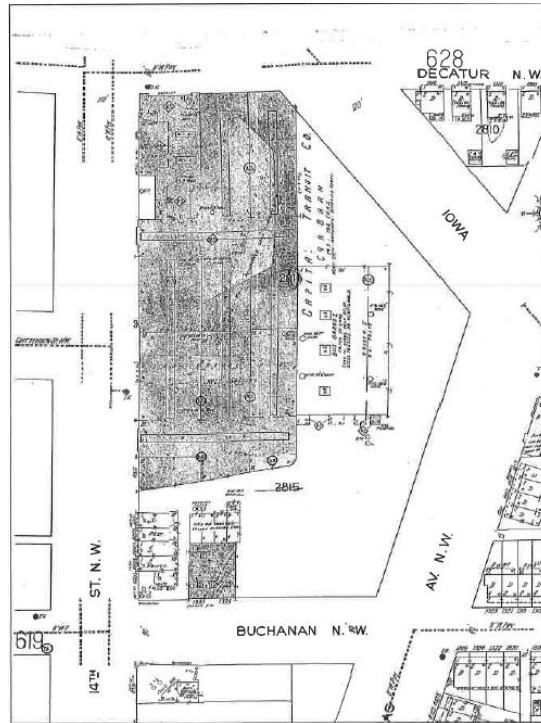


Figure 6. 1974 photograph of the bus garage looking northwest; the 1926 addition is visible on the right (WMATA Archive, George Washington University Special Collections)



The date of the construction of the smokestack at the south elevation of the garage is unknown, but photographs indicate it was constructed after 1914 and before 1962, likely dating to the period of significance. It was constructed to exhaust smoke from the coal-powered boiler room located in the lower level, as discerned from 1978 renovation drawings. (See **Figure 7**) In the 1980s, it was altered with new openings to accommodate updated mechanical equipment.

Another instance of unknown alteration occurred to the north of the tower. 1914 blueprints show that the north elevation of the tower adjacent to the streetcar opening was originally exposed, however, 1978 existing condition drawings show that an angled wall had been built at the streetcar opening, closing off the north elevation of the tower. Today, the wall is still extant, and a doorway has been inserted. (See **Figure 8**)

Figure 7. 1978 Renovation drawing; red arrow shows the smokestack adjacent to the boiler room and coal storage (WMATA)

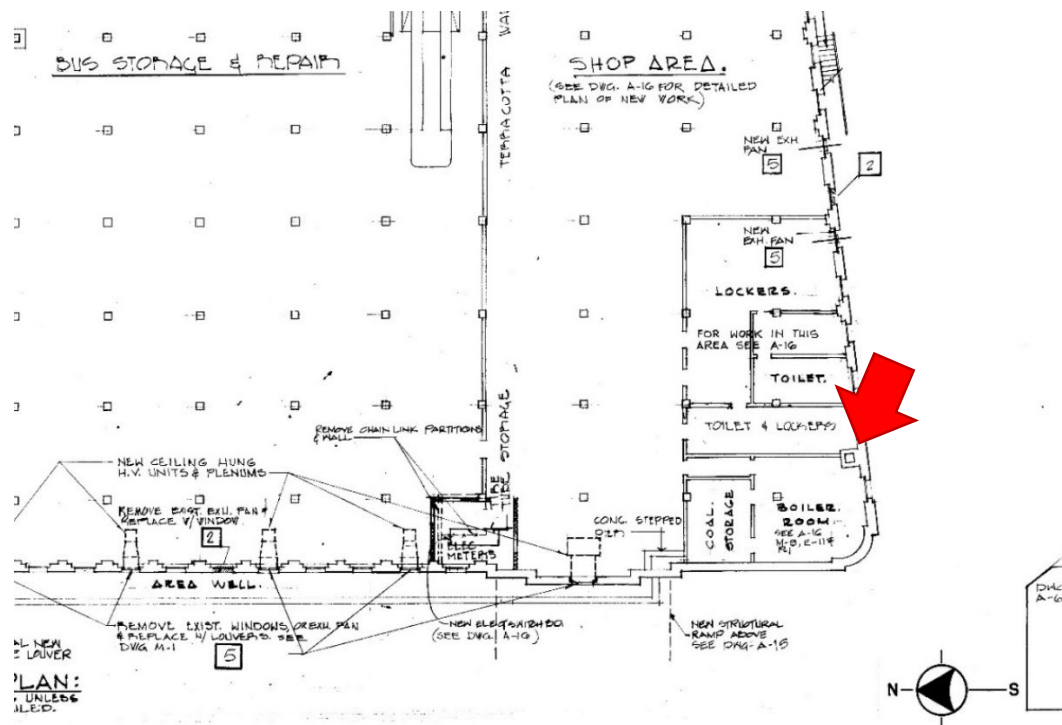


Figure 8. Angled brick wall on right constructed sometime after 1914 and prior to 1978. The wall created an additional interior room and enclosed the north elevation of the tower at that level. The other angled wall with the overhanging door was added after 1978 (BBB)



Numerous significant alterations to the property occurred in the 1980s. To reduce noise effects from bus operations to the surrounding neighborhoods, a thirteen-foot high red brick wall was erected in 1982-1983 around the WMATA property, encircling nearly all of Squares 2811 and 2815, except for the southwest corner at Buchanan and 14th Streets.

From 1987 to 1992, the bus garage underwent a phased renovation and addition. The 1926 bus garage addition to the east of the original structure was demolished, and a one-story maintenance facility and garage with rooftop parking was constructed within the 1983 property wall, wrapping the east, south, and north elevations of the building. (See **Figure 9**) The majority of the original roof was demolished and rebuilt except for roofing over several bays at the northern end and a bay that remained along the full perimeter of the building. (See **Figure 10** and **Figure 11**) All but two of the original wood windows were replaced with aluminum windows, and several window openings were enclosed or changed to bus openings. A bus entry was inserted at the southern end of the west façade, immediately left (north) of the pedimented parapet at that end. (See **Figure 12**) To the right (south) of the pediment, an additional window was changed to a doorway. (See **Figure 13**)

The north and majority of the east elevations of the building were enclosed by the addition and bus ramp. The ramp descends west to east and north to south, following the topography of the site. As a result, Decatur Street, between 14th Street and Iowa Ave. was closed to traffic and was incorporated into the bus garage. A truncated roof encloses the original north elevation of garage. The original north elevation, which featured arched window openings was significantly altered with new bus openings, allowing buses to easily move from the garage to the bus ramp and exit at Decatur and 14th Streets. (See **Figure 14**) The east elevation was enveloped by the addition. The original arched window openings are

still present, although many of the fanlights have been infilled with brick or replaced with louvers. The original east wall remains visible at the upper level interior and from the roof of the 1987 addition. (See **Figure 15**) The addition also had significant effects to the south elevation of the original structure. The lower level of the south elevation was fully enclosed and many of the windows at the upper level were removed and filled with glass block.

Figure 9. Contemporary aerial image of the bus garage looking southeast; the original bus garage was wrapped in the one-story 1987-1992 addition that enclosed Decatur Street to the left (north) of the administrative offices and provided WMATA with rooftop parking at the south and east (Google)



Figure 10. 1987 photograph looking east showing the extent of the demolition that occurred within the bus garage. The entire ceiling and roof structure in this area was removed except for one bay along the perimeter of the east wall (WMATA Archive, George Washington University Special Collections)



Figure 11. Diagram showing the extent of the original roof structure that was removed during the 1987-1992 renovation in green. The red outline shows the original footprint of the garage and the blue outline shows the contemporary property outline. Everything outside the red outline was added during the 1987-1992 renovation (BBB)



Figure 12. Bus entry at the southern end of the west façade was added during the 1987-1992 renovation (BBB)



Figure 13. The two doorways to the right of the pedimented bay are not historic. The one on the left was changed from a window opening to a door prior to 1974 while the one on the right was changed during the 1987-1992 renovation (BBB)



Figure 14. A truncated roof shelters the original north elevation and Decatur Street. Large openings for buses were punched in the wall during the 1987-1992 renovation (BBB)



Figure 15. Roof of the 1987 addition abuts the original east elevation. Several of the arched windows have been infilled with brick (BBB)



Significant interior alterations were made to the administrative offices. Rooms were reconfigured, and a new stair and elevator tower addition was constructed at the north end of the office building. The stair and elevator tower was designed to match the Italian Renaissance Revival style of the rest of the building and features the same materials, a slate hipped roof, overhanging eaves with brackets, and similar brick detailing. The interior of the garage was also impacted. The majority of columns on the upper level were removed and reconstructed when the majority of the roof was demolished and rebuilt. On the lower level, the original columns and ceiling slab remain, however, the concrete floor slab was removed, and the floor was excavated approximately 12 inches and re-laid. The original columns and exterior walls are supported by non-historic concrete footings to adjust for the lowered floor. (See **Figure 16**)

Figure 16. Lower level of the bus garage; the original columns and ceiling are present, however, the concrete floor was removed and excavated in the 1980s. The new concrete footings below the columns are visible in the photo (BBB)



Summary of Exterior Conditions

The administrative offices and 14th Street façade exterior building fabric are in overall fair condition. Open and debonded masonry joints are present but are concentrated to vertical facing joints at the building cornices, projecting string courses, and sills. The stone and brick masonry exhibit limited spalling, cracks, perforations from ferrous metal inserts or previous attachments, inappropriate past masonry repairs and patches, soiling, and biological growth. Cracks and spalling are especially present at the stone cornice and the stone surrounds at the original streetcar openings along 14th Street. (See **Figure 17**) The pebble-dashed stucco material present at the eaves of the administrative offices and tower is in good to fair condition, exhibiting some areas of cracking and missing stucco. Many slates on the hipped roof are broken or loose. Metal snow guards are bent and ineffective and the construction of the slate roof shows deficiencies. The roof should be investigated for appropriate flashing, slate headlap, underlayment, and ridge construction. The roof may require replacement.

Repair and restoration of the administrative offices and 14th Street NW masonry façade will require a variety of treatments. Cracks should be repaired and patched with grout or restoration mortar with a composition appropriate for the masonry substrate. Structural cracks may require the insertion of pins to further stabilize the masonry. Small spalls may be tooled to sound stone so that further spalling doesn't occur, and that water doesn't collect or pool. Larger spalls may require patching with restoration mortar or full or partial masonry replacement. All open and debonded joints should be repointed using matching mortar and missing masonry patched with matching materials. Ivy plants growing on the masonry should be carefully removed. The masonry should be cleaned using the gentlest means possible to remove soiling, staining, and biological growth. Soiling is especially apparent at the cornice and at the base of the building.

Figure 17. The north facing streetcar/bus opening adjacent to the tower exhibits stone cracking, spalling, and masonry soiling which require repair (BBB)



Summary of Treatment and Effects to the Historic Fabric

As discussed above, the bus garage has experienced many alterations across its 114-year history, especially as a result of the 1987-1992 renovation. Such changes have affected the integrity of the historic fabric. The 14th Street façade has been altered the least and retains much of its original Italian Renaissance Revival design. The façade, including the administrative offices and tower, has a high level of integrity of design, materials, and workmanship. The remaining elevations have been significantly modified and the integrity of design, materials, and workmanship has been diminished. The same can be said for the interior of the garage, which was significantly altered by the removal of the majority of the upper level columns, lower level slab, and roof structure.

The drawing below shows the existing historic masonry walls overlaid on the design for the upper level of the new bus garage. (See **Figure 18**) Due to the alterations of the historic fabric and the need for a new bus garage that can accommodate efficient and safe vehicle circulation for 40'-0" and 60'-0" articulated buses, the existing bus garage must be replaced. The new bus garage will also ensure adequate height clearance for newer diesel buses and future overhead charging for electric buses, be reorganized to expand the number of maintenance bays and bus storage parking, incorporate a retail element for increased community integration, will be able to 100 percent filter exhaust air, and will reduce operating costs through sustainable strategies. The replacement bus garage project proposes that the east wall and the majority of the north and south walls be demolished. However, the entire west façade, including the administrative offices and tower, would be retained and preserved allowing for the conservation, repair, and cleaning of areas of damage, weathering, soiling, and staining. There is also the opportunity to replace the existing widows with replicas of the historic windows and restore window openings that were previously infilled or replaced with louvers. Such treatments would be developed as design coordination for the project continues. Portions of the upper level of the north and

south elevations, immediately adjacent to the west façade, may be retained but will require continued design coordination and input from the Section 106 process and other review processes, before a final decision on treatment can be made.

Bibliography

Primary Resources

D.C. History Center

The D.C. History Center, located at the Carnegie Library in Washington, D.C. holds several photograph collections, including the John P. Wymer collection, Kathleen Sinclair Wood collection, the Crockett streetcar photo collection, and the Joseph Jessel streetcar slide collection, which had several photographs of the Northern Bus Garage from the 1940s through the 1960s. The Capital Transit Company records are also located at the History Center, which included photographs and blueprint drawings from 1914.

WMATA Archive at the George Washington University Special Collections Library

The WMATA Archives at the George Washington University Special Collections Library held many photographs of the bus garage from 1974 and of the 1987-1992 renovation and addition work.

Secondary Resources

National Register of Historic Places, Capital Traction Company Car Barn, Washington, D.C., National Register #13000290

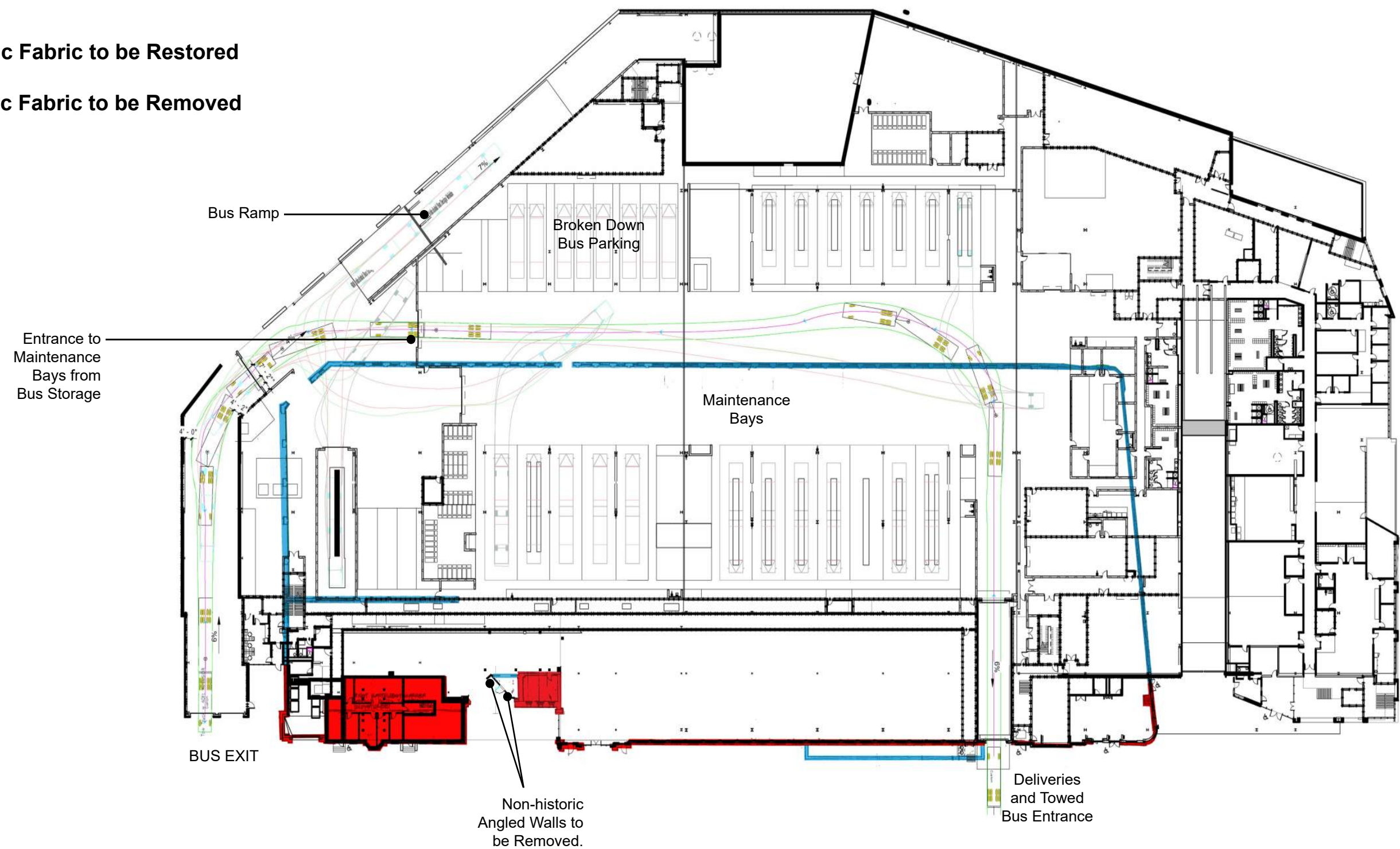
National Register of Historic Places, Streetcar and Bus Resources of Washington, D.C. Multiple Property Listing, Washington, D.C., #64500948

Northern Bus Garage

Proposed Floor Plan with Historic Overlay – Upper Level



- Historic Fabric to be Restored
- Historic Fabric to be Removed



ATTACHMENT 4
RESTORATION NARRATIVE SCOPE OF WORK, ELEVATIONS AND PLANS
NORTHERN BUS GARAGE RENOVATION PROJECT
MEMORANDUM OF AGREEMENT

Restoration Narrative Scope of Work

The restoration scope for the WMATA Northern Bus Garage will include the restoration of the 14th Street elevation; a 36'8" portion of the original south elevation, including the chimney; and a 28' 7" portion of the original north elevation. The restoration will include the removal of non-original alterations, including the c. 1987-1992 Administration Building stair tower, the 1970s angled brick wall in the original streetcar entry, two non-original pedestrian doors in the 14th Street elevation (northern door c. 1970, southern door c. 1987-1992), and the removal of non-original brick window infills. The elevations will be cleaned, repaired, and repointed where needed. The 14th Street NW elevation will be supported by temporary supports during excavation and construction of the new facility. The south portion of the elevation that will be retained will be catalogued, dismantled, and reassembled prior to restoration as its foundations are in conflict with the new bus drive aisle.

The elevation restoration includes the installation of new aluminum wrapped wood core IGU windows and exterior Administration Building doors to match the historic windows and doors as closely as is possible. Historic images, such as photographs and available plans, were used as source material for the design of new doors and windows. The historic symmetrical design of the doors will be retained for the new doors, in keeping with the historic character of the building. The two extant original wood windows on the 14th Street NW elevation will be restored and reinstalled in their existing locations. A historic round wood window currently located at the east elevation will be salvaged, restored, and installed in an opening in the 14th Street NW elevation where this same type of window was originally located, but the window was removed and bricked in at some point.

A survey completed in February of 2020 determined that overall, the brick masonry is in good condition. There are limited areas of step cracking, bio growth, staining, incompatible repointing, and previous alterations. All historic fabric will be cleaned in a manner consistent with the Secretary of the Interior's Guidelines for Rehabilitation: cleaning soiled masonry surfaces with the gentlest method possible. Non-original brick or mortar will be removed. Non-original and deteriorated mortar will be removed and replaced with an approved matching mortar as noted above. Non-original brick will be replaced with historic brick salvaged from the site and mortar analysis will be undertaken to determine an acceptable mortar for repairs. In order to retain as much historic masonry in situ as possible, small brick cracks or mechanical damage will be repaired rather than replaced. These repairs are ONLY for minor cracks and holes from anchors drilled in the face of the brick will be repaired with a patching mortar in compliance with the Secretary of the Interior's Guidelines for Rehabilitation.

The limestone and granite portions of the elevation are in a more distressed condition than the brick and will require more repair and, in select locations all noted on the drawings, replacement to match historic. Small areas of stone damage will be repaired with custom matched mortar repairs or dutchman. Only in a few limited instances will replacement to match historic be required. All limestone and granite will be cleaned in a manner consistent with the Secretary of the Interior's Guidelines for Rehabilitation: cleaning soiled masonry surfaces with the gentlest method possible. As detailed in contract documents, the non-historic parapet flashing currently installed in some locations on 14th Street will be removed to expose the historic limestone beneath. Small areas of stone damage will be repaired with custom matched mortar repairs or dutchman. Only in a few limited instances will replacement to match historic be required.

The pebble dash stucco at the cornice of the Administration Building and Tower will be cleaned in a manner consistent with the Secretary of the Interior's Guidelines for Rehabilitation: cleaning soiled masonry surfaces with the gentlest method possible. Repairs are identified in the contract documents where cracking and de-laminating has occurred. The painted wood trim in the cornice will be cleaned, repaired, and repainted.

The restoration will include the replacement of the non-original Administration Building and Tower slate and metal roofs with historically appropriate slate and metal roofing. The roofs and underlayment require full replacement based on poor condition. New gutters and downspouts to match the historic will be installed.

Restoration Elevations and Plans on following pages:



1914 Photograph (DC History Center)

WMATA Northern Bus Garage

4615 14th Street NW
Washington, D.C. 20011

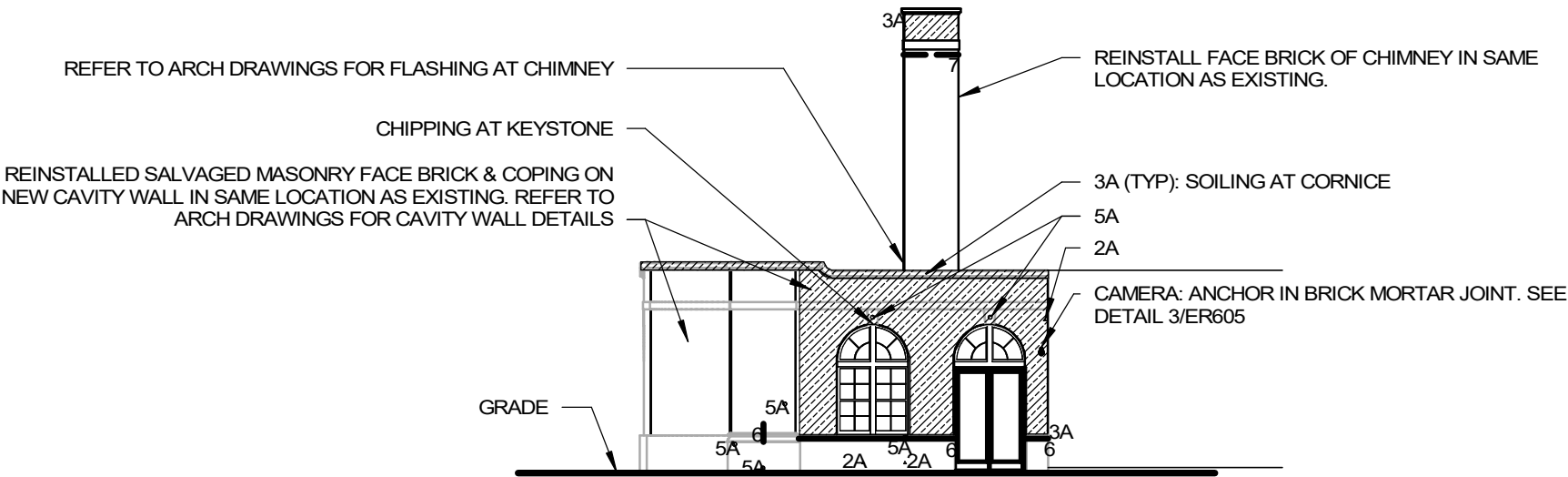
Elevations for Section 106 Consultation
October 1, 2021

WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY

PREPARED BY:

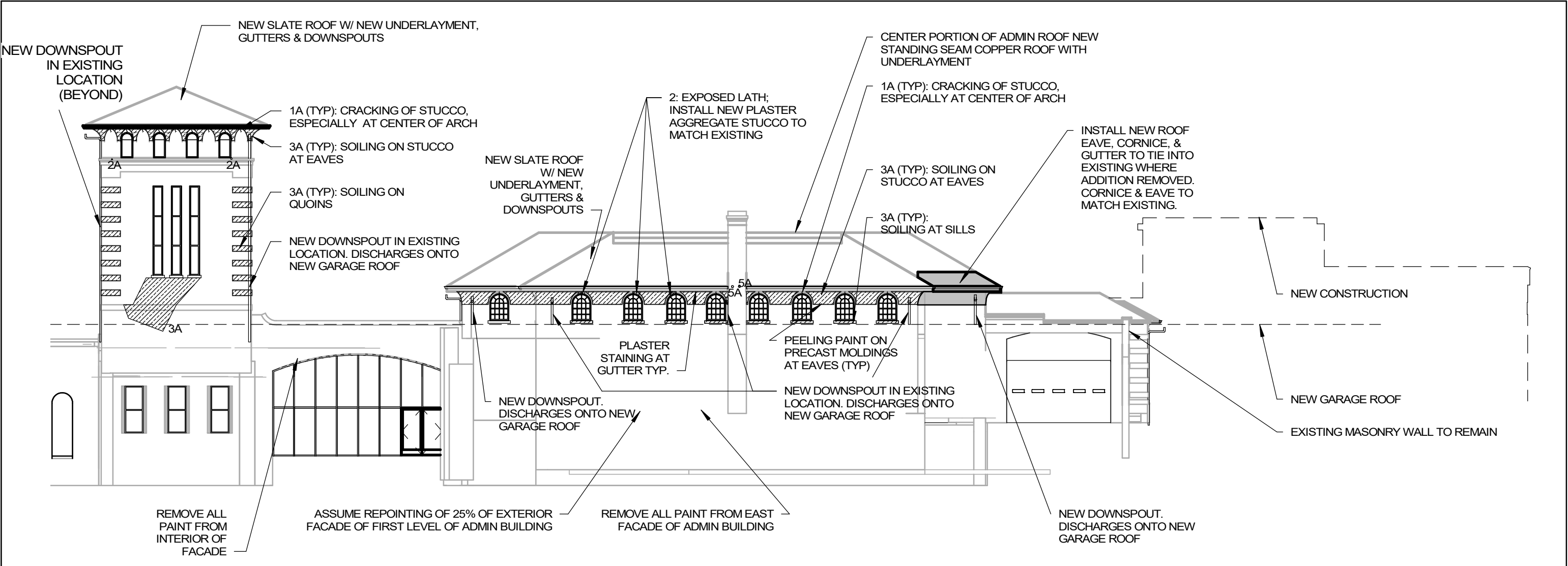
BEYER BLINDER BELLE ARCHITECTS & PLANNERS LLP
3307 M STREET NW
WASHINGTON, D.C. 20007

Northern Bus Garage - Preservation Treatment Approach - Elevations



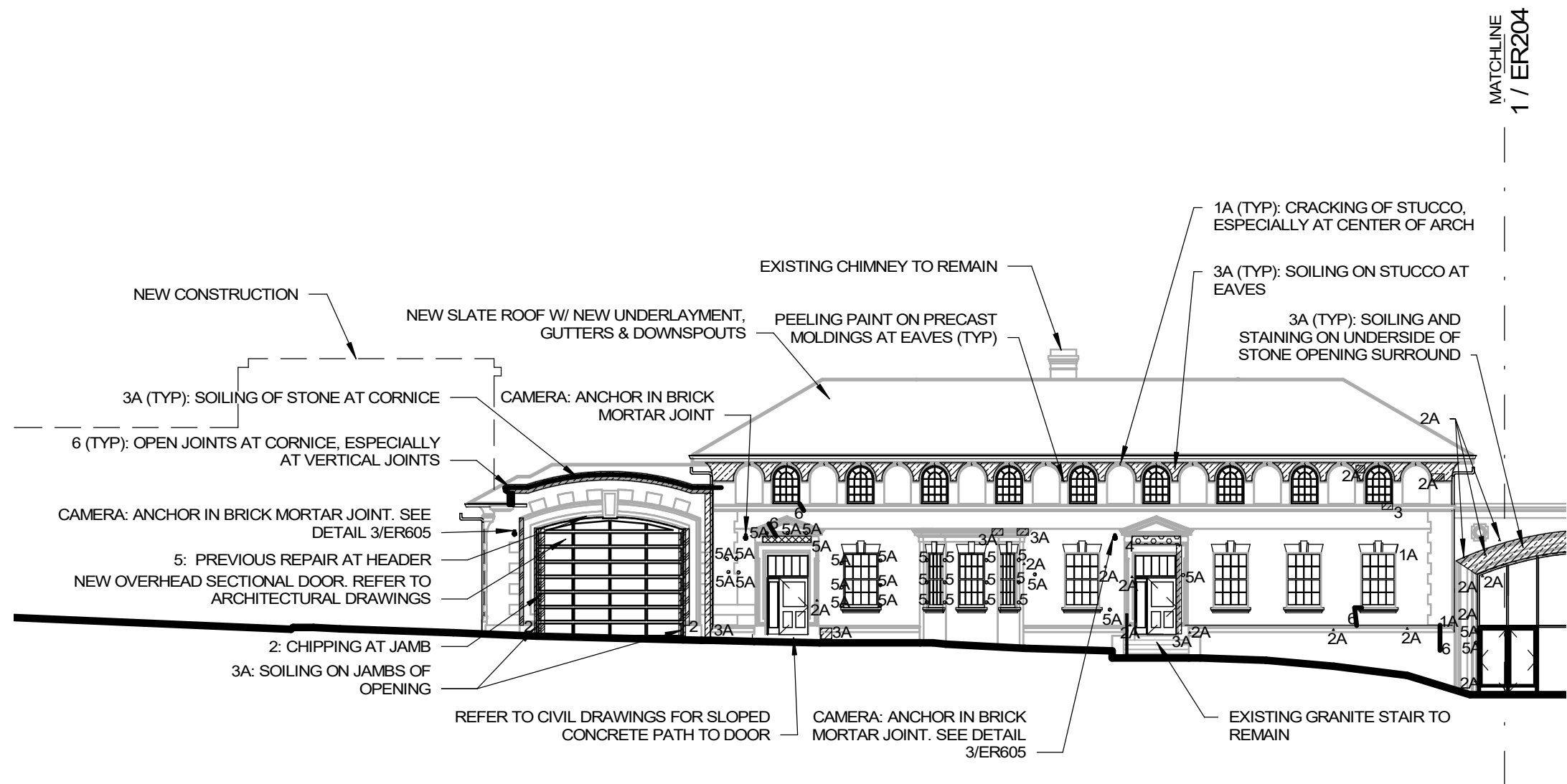
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Northern Bus Garage - Preservation Treatment Approach - Elevations



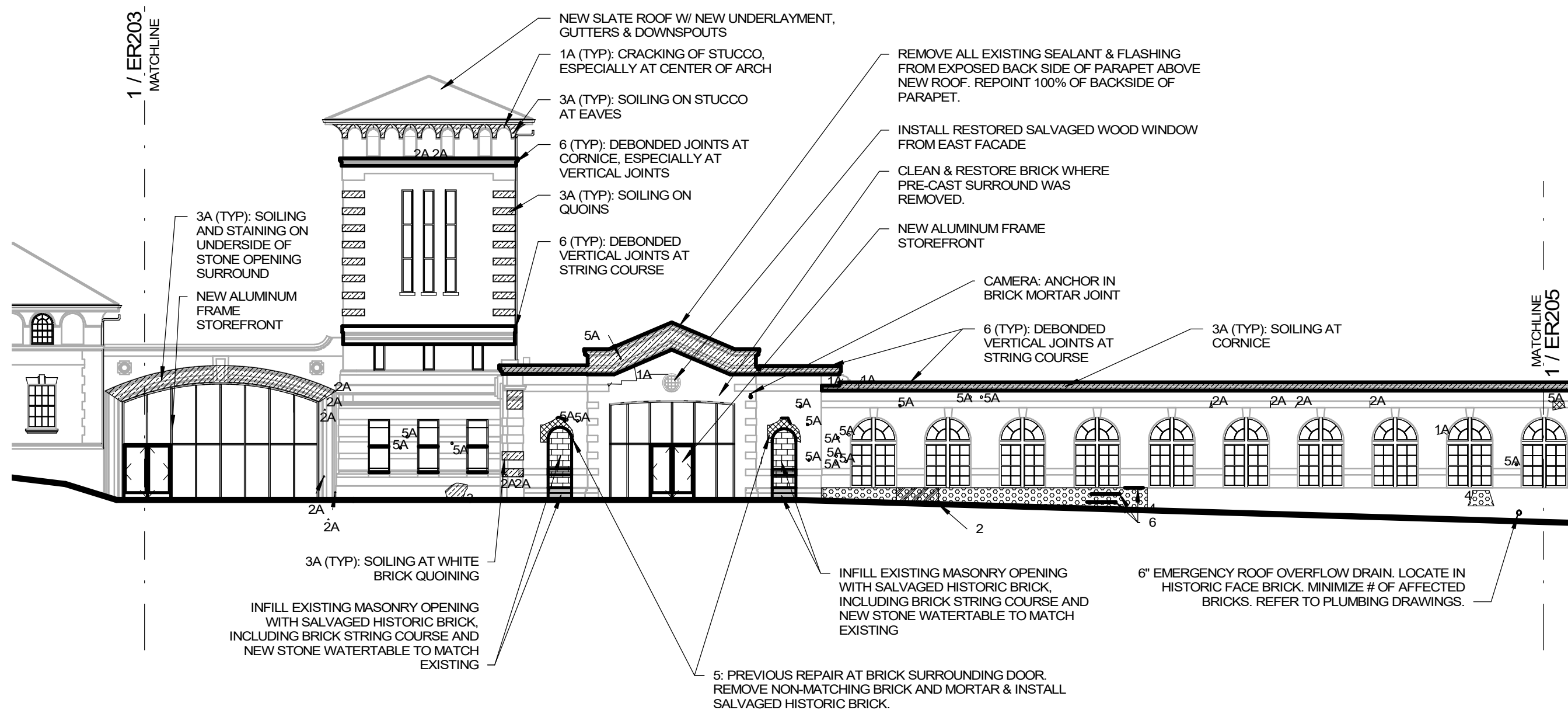
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







Northern Bus Garage - Preservation Treatment Approach - Elevations



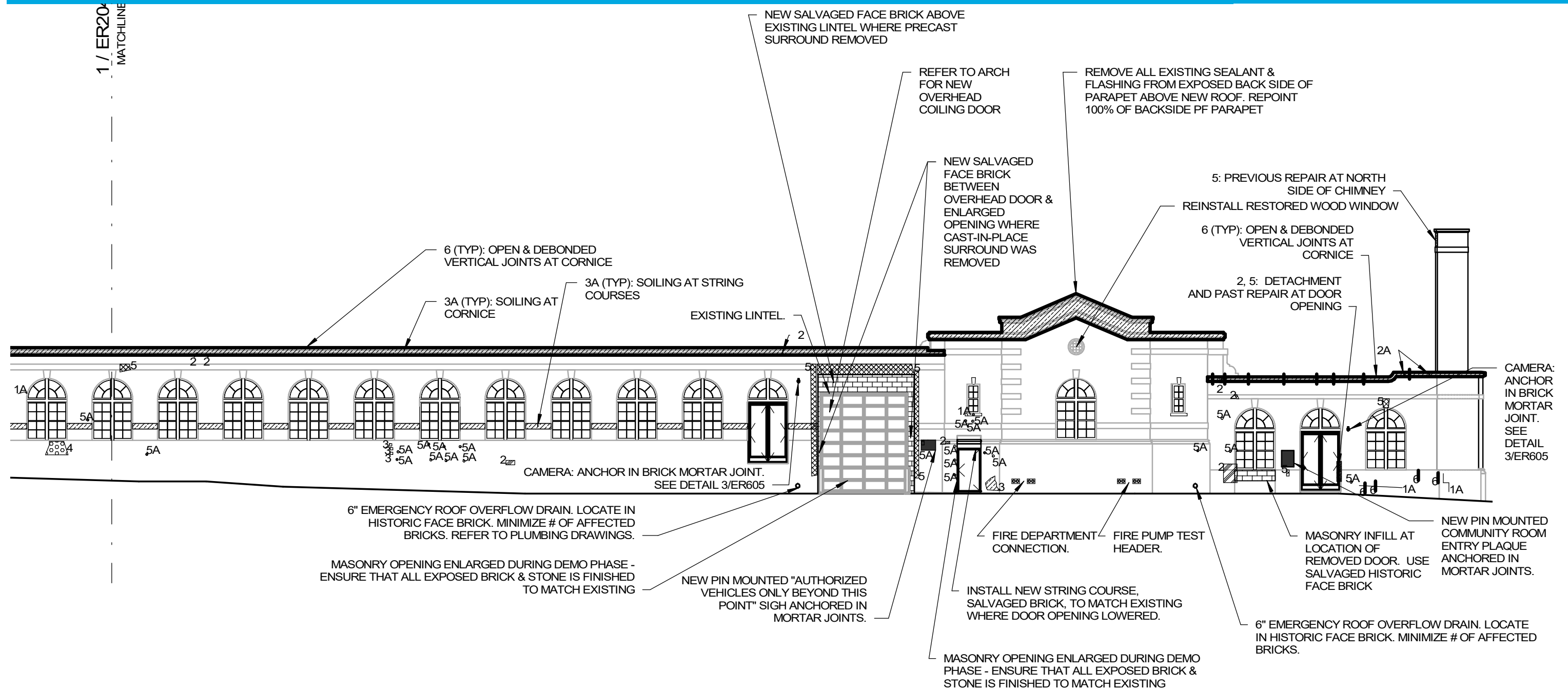
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Northern Bus Garage - Preservation Treatment Approach - Elevations



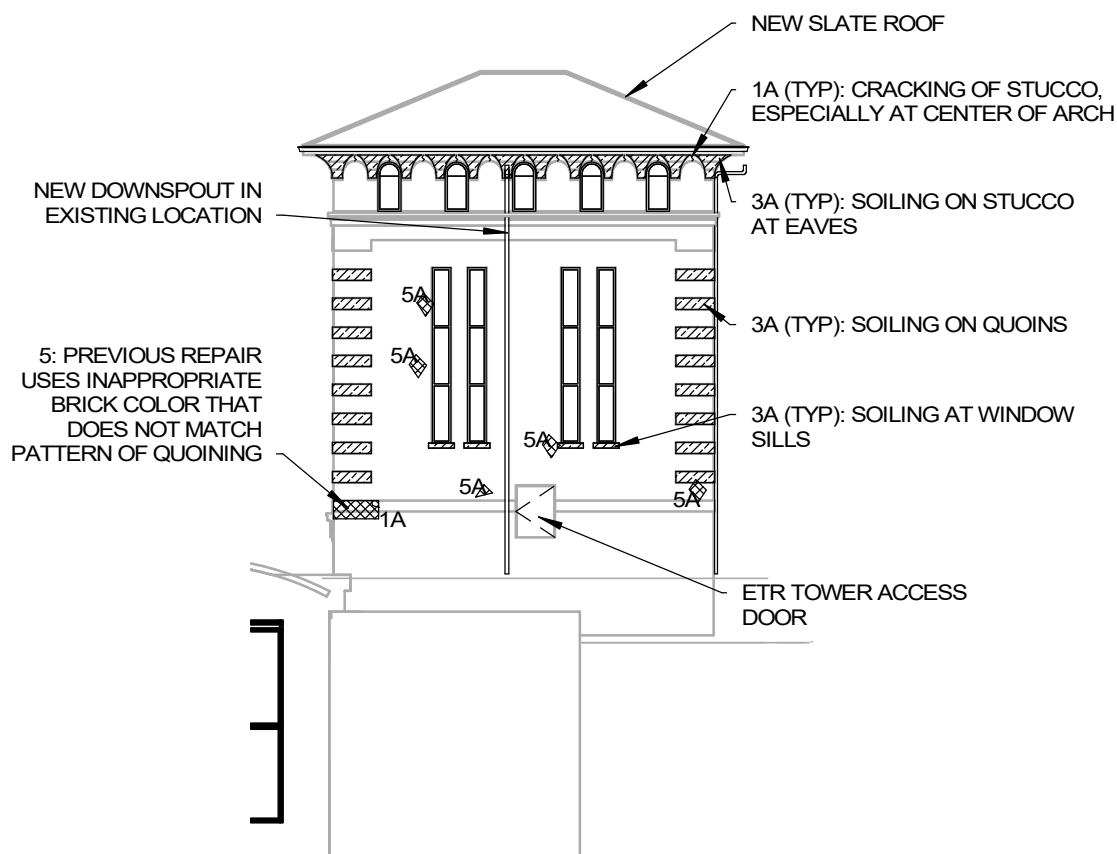
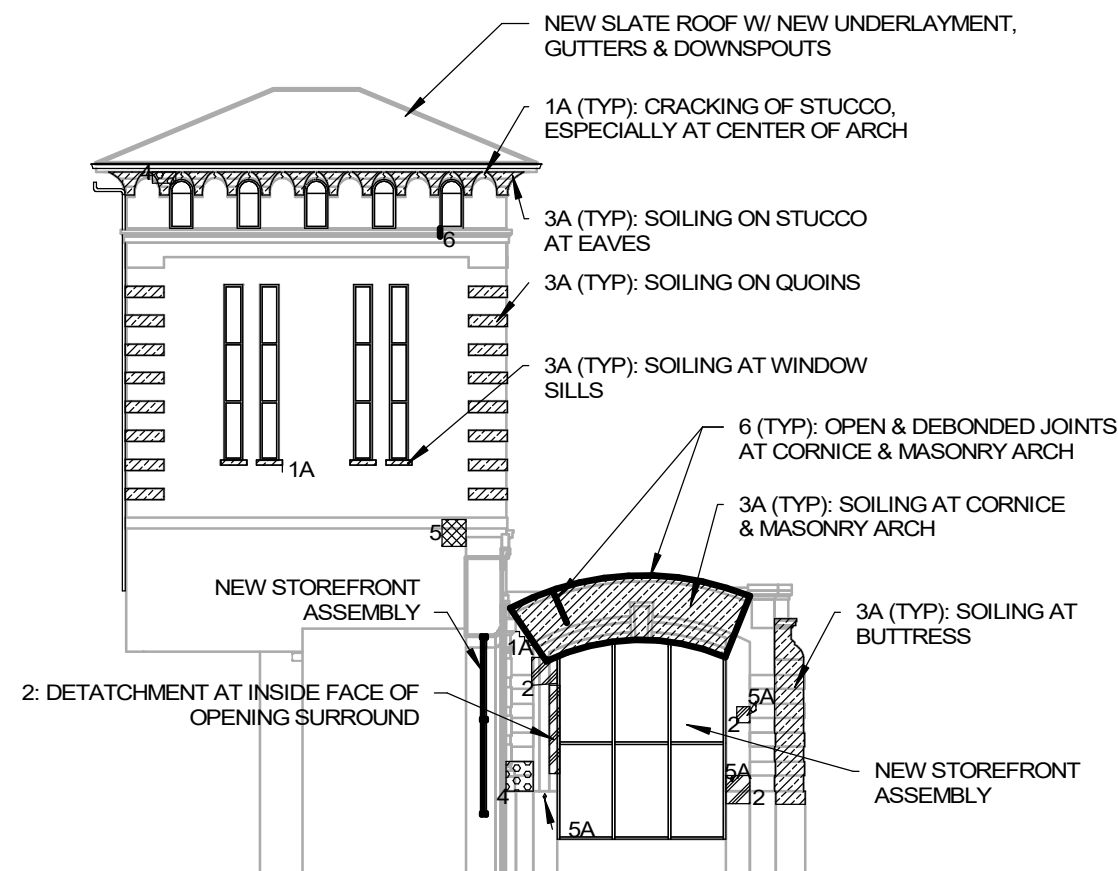
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Northern Bus Garage - Preservation Treatment Approach - Elevations



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<div><div><div></div><div>EXISTING</div></div><div><div></div><div>NEW OR REINSTALLED</div></div><div><div></div><div>SALVAGED HISTORIC FABRIC</div></div></div> <div>EXISTING CONDITIONS REQUIRING TREATMENT</div> <div><div><div></div><div>1 - REPAIR CRACKS AND DEFORMATION</div><div>1A - CRACK</div></div><div><div></div><div>2 - REPAIR DETACHMENT AND DISLOCATION</div><div>2A - CHIPPING</div></div><div><div></div><div>3 - CLEAN DISCOLORATION AND DEPOSITS</div><div>3A - SOILING</div></div><div><div></div><div>4 - REMOVE BIOLOGICAL COLONIZATION</div></div></div>	<div><div><div></div><div>5 - ALTERATIONS/PREVIOUS REPAIRS. REMOVE NON-ORIGINAL BRICK OR STONE & INSTALL SALVAGED HISTORIC BRICK OR NEW STONE TO MATCH EXISTING.</div><div>5A - FERROUS METAL INSERTS</div></div><div><div></div><div>6 - OPEN JOINT</div><div>7 - DEBONDED JOINT</div></div><div><div></div><div>8 - INACCESSIBLE PORTION OF FACADE DUE TO EXISTING CONSTRUCTION. BBB TO SURVEY AFTER DEMOLITION</div></div></div> <div>REFER TO BRICK AND STONE REPAIR, REPOINTING AND CLEANING SPECIFICATIONS FOR TREATMENT PROCEDURES</div>	<div><div><div></div><div>NEW ALUMINUM WRAPPED WOOD CORE WINDOW WITH IGU</div><div>RE: ER-601 FOR WINDOW SCHEDULE</div></div><div><div></div><div>REINSTALLED RESTORED HISTORIC WOOD WINDOW</div><div>RE: ER-601 FOR WINDOW SCHEDULE</div></div><div><div></div><div>NEW DOOR TAG</div></div><div><div></div><div>WINDOW TAG</div></div></div>	<div><div><div>1. INSTALL NEW SLATE ROOF AND UNDERLAYMENT AT TOWER, ADMINISTRATION BUILDING, & HIPPED ROOF TO NORTH OF ADMINISTRATION BUILDING TO REMAIN. REFER TO ARCH DRAWINGS FOR SPLASH BLOCKS</div><div>2. ALL MASONRY TO RECEIVE GENERAL CLEANING. REFER TO MASONRY CLEANING SPECS</div><div>3. REMOVE ALL EXISTING PARAPET FLASHING . REPOINT ALL SKYWARD FACING JOINTS IN PARAPET & INSTALL LEAD T'S AT ALL HORIZONTAL JOINTS IN EXISTING LIMESTONE COPING</div></div><div><div><div>1</div><div>ER101</div></div><div>EL-03-03 WEST ELEVATION - EXTERIOR RESTORATION</div><div>1/16" = 1'-0"</div></div></div>	

Northern Bus Garage - Preservation Treatment Approach - Elevations



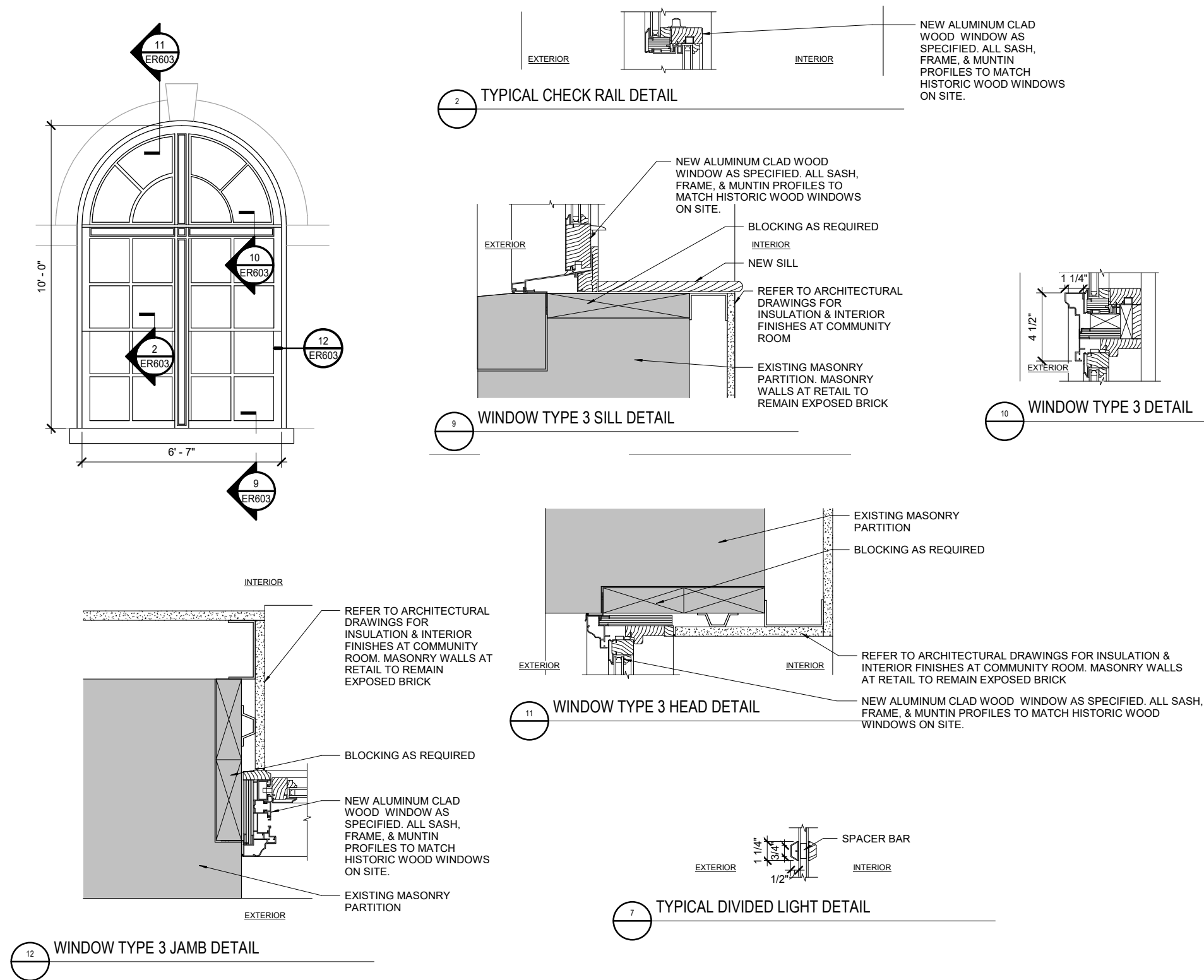
2 EL-04-02 TOWER NORTH ELEVATION - EXTERIOR RESTORATION
ER102 1/16" = 1'-0"

4 EL-01-02 TOWER SOUTH ELEVATION - EXTERIOR RESTORATION
ER102 1/16" = 1'-0"

SYMBOLS LEGEND			SHEET	NOTES
<div>—</div> EXISTING <div>—</div> NEW OR REINSTALLED SALVAGED HISTORIC FABRIC EXISTING CONDITIONS REQUIRING TREATMENT <div>⚡</div> 1 - REPAIR CRACKS AND DEFORMATION 1A - CRACK <div>▨</div> 2 - REPAIR DETACHMENT AND DISLOCATION 2A - CHIPPING <div>▩</div> 3 - CLEAN DISCOLORATION AND DEPOSITS 3A - SOILING <div>⦿</div> 4 - REMOVE BIOLOGICAL COLONIZATION	<div>▨</div> 5 - ALTERATIONS/PREVIOUS REPAIRS. REMOVE NON-ORIGINAL BRICK OR STONE & INSTALL SALVAGED HISTORIC BRICK OR NEW STONE TO MATCH EXISTING. 5A - FERROUS METAL INSERTS <div>—</div> 6 - OPEN JOINT <div>---</div> 7 - DEBONDED JOINT <div>⦿</div> 8 - INACCESSIBLE PORTION OF FACADE DUE TO EXISTING CONSTRUCTION. BBB TO SURVEY AFTER DEMOLITION REFER TO BRICK AND STONE REPAIR, REPOINTING AND CLEANING SPECIFICATIONS FOR TREATMENT PROCEDURES	<div>🏠</div> NEW ALUMINUM WRAPPED WOOD CORE WINDOW WITH IGU RE: ER-601 FOR WINDOW SCHEDULE <div>🏠</div> REINSTALLED RESTORED HISTORIC WOOD WINDOW RE: ER-601 FOR WINDOW SCHEDULE <div>ER-01</div> NEW DOOR TAG <div>FS01</div> WINDOW TAG	4	1. INSTALL NEW SLATE ROOF AND UNDERLAYMENT AT TOWER, ADMINISTRATION BUILDING, & HIPPED ROOF TO NORTH OF ADMINISTRATION BUILDING TO REMAIN. REFER TO ARCH DRAWINGS FOR SPLASH BLOCKS 2. ALL MASONRY TO RECEIVE GENERAL CLEANING. REFER TO MASONRY CLEANING SPECS 3. REMOVE ALL EXISTING PARAPET FLASHING . REPOINT ALL SKYWARD FACING JOINTS IN PARAPET & INSTALL LEAD T'S AT ALL HORIZONTAL JOINTS IN EXISTING LIMESTONE COPING

Northern Bus Garage - Preservation Treatment Approach - Windows

Window Type 3

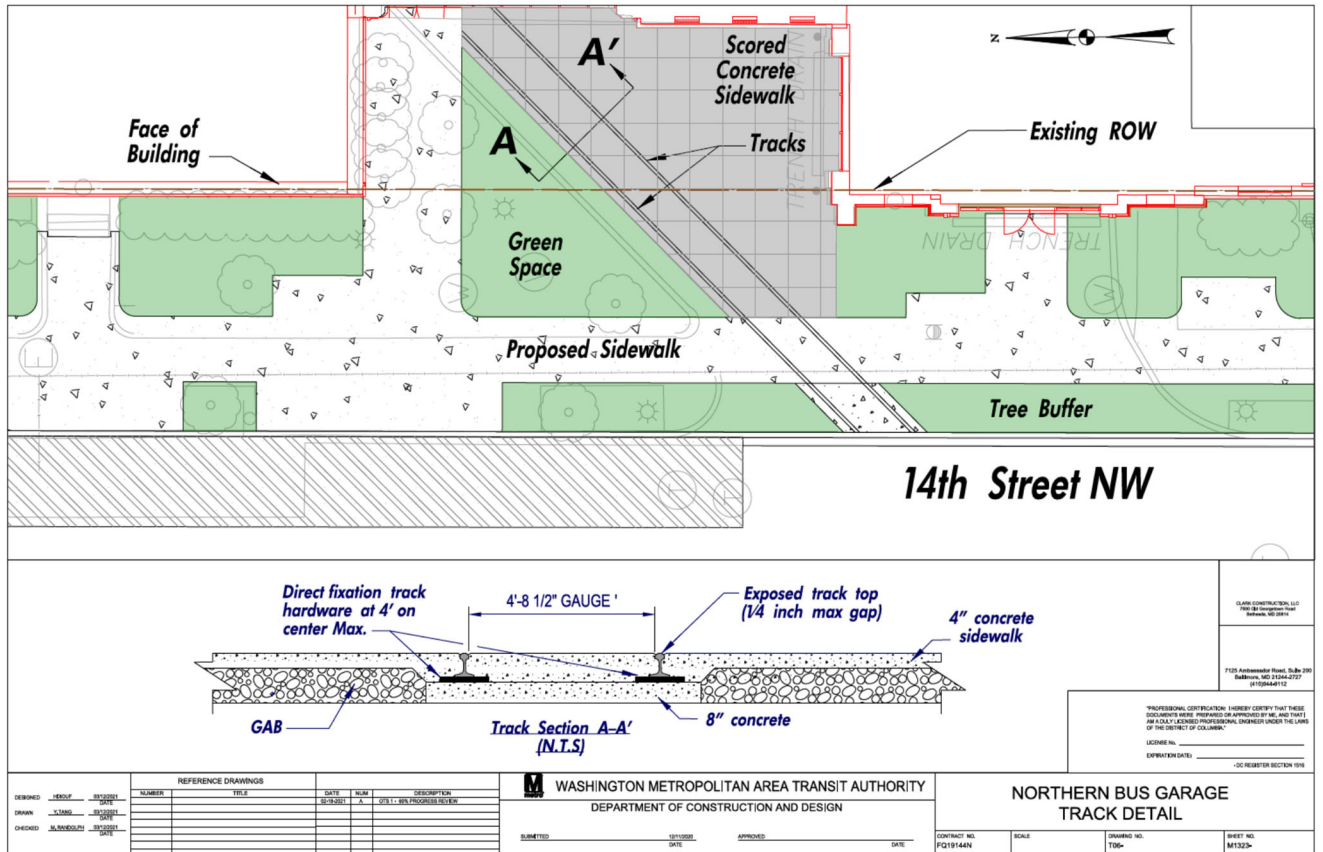


Remaining Original Window



All sash, frame, and muntin profiles to match the only remaining original window on site.

ATTACHMENT 5 **REPLICA STREETCAR TRACK INSTALLATION** **NORTHERN BUS GARAGE RENOVATION PROJECT** **MEMORANDUM OF AGREEMENT**



ATTACHMENT 6
INTERPRETIVE SIGNAGE EXHIBITS
NORTHERN BUS GARAGE RENOVATION PROJECT
MEMORANDUM OF AGREEMENT

To help mitigate the adverse effects associated with the renovation of the Northern Bus Garage, WMATA will develop and install interpretive signage exhibits as described below. This Scope of Work is organized into four sections: Background, Goals of the Exhibits, Tasks, and Deliverables.

Background:

WMATA plans to renovate the Northern Bus Garage, which is listed in the National Register of Historic Places (NRHP; NR# 13000290 listed April 5, 2013) and as a DC Historic Landmark (September 27, 2012) as the Capital Traction Company Decatur Street Car Barn. The renovation effort will remove portions of the historic fabric of the car barn, which will result in an adverse effect.

As part of mitigation efforts for the adverse effect, WMATA will be providing interpretive signage exhibits as explained below. Exterior signage shall focus on the historical and architectural characteristics (the building's history, architecture, and use) that qualify the building for listing in the NRHP. Interior exhibits will provide additional details about the Northern Bus Garage and related topics such as the role the garage played in the development of the surrounding neighborhood and community.

Goal of the Exhibits:

These interpretive signage exhibits will explain the historical and architectural characteristics that qualify the Northern Bus Garage for listing in the NRHP and connect the community and others to the significance of the Northern Bus Garage, especially the restored portions of the 1906 building along 14th Street, NW, by explaining the role the facility played in the development of transportation in Washington, D.C. and the surrounding neighborhood. Broader topics related to commercial development, social history, African American history, and other themes associated with the facility and the community will also be addressed in the community room exhibits to provide relevant information from a wider variety of perspectives. All exhibits will be designed to be compatible with their historic setting, both exterior and interior, and will not cause any damage to historic fabric.

Specific Tasks:

One to three exterior interpretive signage exhibits will be developed to explain the historical and architectural significance of the Northern Bus Garage. Text will be based upon the NRHP nomination for the Capital Traction Company Car Barn, the NRHP Multiple Property Documentation for Streetcar and Bus Resources of Washington, DC 1862-1962, and related research. One exhibit will be used to explain the replica streetcar tracks that will be installed in

front of the Northern Bus Garage along 14th Street, NW. Proposed signage locations will be identified through consultation with the DC SHPO. The primary location of exterior exhibits will be adjacent to the restored portions of the building on 14th Street, NW, but additional exhibits may also be installed adjacent to and/or on newly constructed portions of the Northern Bus Garage to provide additional interpretive opportunities and to enliven and break down the scale of the large new building. The appearance of the exterior exhibits, especially those along 14th Street, NW and within or adjacent to public space, will be based upon existing interpretive signage exhibits within the District of Columbia (e.g. the Neighborhood Heritage Trails installed by Cultural Tourism DC and/or the Kalorama Citizens Association signage – see examples below) to provide consistency throughout the city and make it easier for users to recognize the as interpretive signage exhibits. Any interpretive signage exhibits that may be attached to the newly constructed portions of the Northern Bus Garage may be designed with greater flexibility.





QTY: 1 (24"x42") EXTERIOR GRAPHIC FOR PEDESTAL

SCALE: 3/4"=1'-0"

MATERIAL: EXTERIOR GRADE CHPL GRAPHIC FOR
SURFACE MOUNT ON PEDESTAL



Up to five interior interpretive signage exhibits will be installed in the 1600 sq. ft. community room which, for reference, has a finished wall height of 13 ft. 8 in. The interior exhibits shall focus on broader historical themes that relate to the development of the Northern Bus Garage and the surrounding neighborhood and community, including African-American History and related topics. The content will be determined in consultation with the DC SHPO and the consulting parties; the final number of exhibits will be determined in consultation with FTA and DC SHPO. The appearance of the interior signs should relate to that of the exterior signage exhibits, but more flexibility can be applied to the design of the interior exhibits provided they do not damage any historic interior fabric. For example, three-dimensional artifacts, audio/visual samples, personal memorabilia, and other creative methods of interpretation may be considered for incorporation into the designs.

Deliverables:

1. In accordance the Section 106 Memorandum of Agreement (MOA) the contractor hired by WMATA will solicit initial input from DC SHPO and the consulting parties regarding the topics they would like to have included in the interpretive signage exhibits. As appropriate to fully develop the topics, the contractor will conduct additional outreach to individuals or groups that are knowledgeable about community history.
2. Based upon the feedback provided in Deliverable 1 above, the contractor will research historical themes using primary and secondary sources. The contractor will conduct a minimum of three oral history interviews with relevant community members and people historically associated with the Northern Bus Garage facility. Oral histories shall be transcribed and transcriptions shall be provided to consulting parties upon request.

3. The contractor will develop draft text and graphics for interpretive signage exhibits, along with recommendations for the locations, size, and related details in keeping with the existing interpretive signage examples cited above.
4. Full color drafts of all interpretive signage exhibits will be provided in digital format to the consulting parties and DC SHPO for review and comment.
5. The contractor shall submit digital versions of the full color drafts and all consulting party comments to the DC SHPO for final review. The contractor will consult further with the DC SHPO to finalize all aspects of the interpretive signage exhibits including but not limited to text, images, location, size and design. Once approved by DC SHPO in writing, the contractor shall prepare final plans and a cost estimate for fabrication and installation of all interpretive signage exhibits.
6. WMATA shall fabricate and install all the interpretive signage exhibits within thirty days of issuance of the building occupancy permit, in accordance with the Section 106 MOA.